

Capital Beltway Project – Mitch Lester

I-95/395 Project – Jeff Cole



Some things can't wait for traffic.

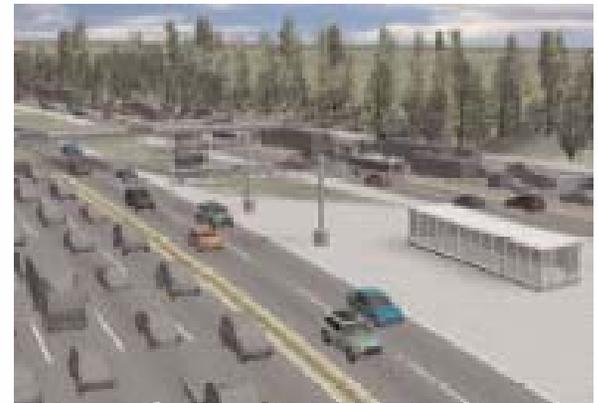
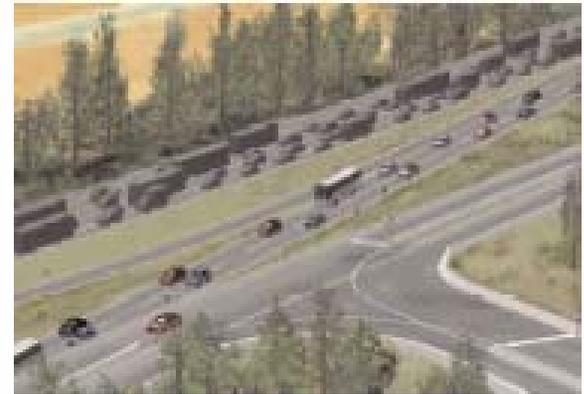
Agenda

- HOT Lanes Overview
- Capital Beltway Project
 - Project Overview
 - Design-Build Process
 - Expectations and Requirements
 - Subcontract Packages
 - Schedule
- I-95/395 Project
 - Project Overview
 - Design-Build Process
 - Expectations and Requirements
 - Subcontract Packages
 - Schedule
- Contacts
- Questions

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What are HOT lanes?

- Free to HOV-3, buses, motorcycles & emergency vehicles
- Motorists can choose to pay a toll when they need a faster, more reliable trip
- Variable toll prices, based on real-time traffic conditions, are used to keep lanes congestion free
- Electronic signs display latest toll rates
- Electronic tolling – no toll booths, stopping or slowing down
- Enhanced safety through technology



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Regional solution

- 70-mile network of HOT Lanes
- Provides congestion relief and new choices
- Expands regional HOV system
- Creates free-flowing network for buses
- Connects HOV on I-95/395 with Capital Beltway, I-66, Dulles Toll Road, and Braddock Road



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Public-private partnership

Delivering vital transportation improvements sooner than traditional funding and procurement methods would allow

<i>Virginia Department of Transportation</i>	<i>Fluor-Transurban</i>
<ul style="list-style-type: none"> • State agency • Ownership • Oversight • Environmental review • Safety & design standards 	<ul style="list-style-type: none"> • Private partners <ul style="list-style-type: none"> – Publicly owned engineering, construction firm – Toll road developer, investor & operator • Funds for construction, operations and maintenance • Builder • Long-term operator • Ongoing, routine maintenance & incident response

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I-495 Capital Beltway Project

Mitch Lester

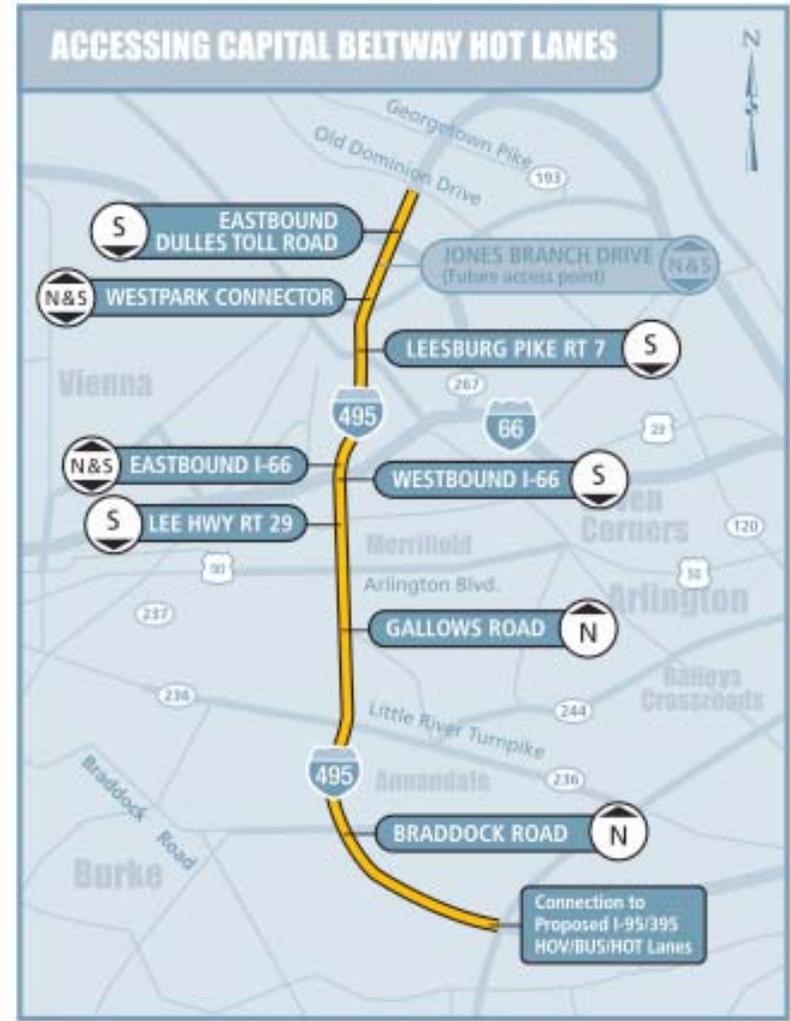
Project Director, Fluor-Lane, LLC



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HOT lanes on the Capital Beltway

- Two new HOT lanes in each direction
- 14 miles from Springfield to north of Dulles Toll Road
- HOV and reliable bus service for the first time
- New or improved soundwalls to reduce impacts on local neighborhoods
- Replacement of aging infrastructure

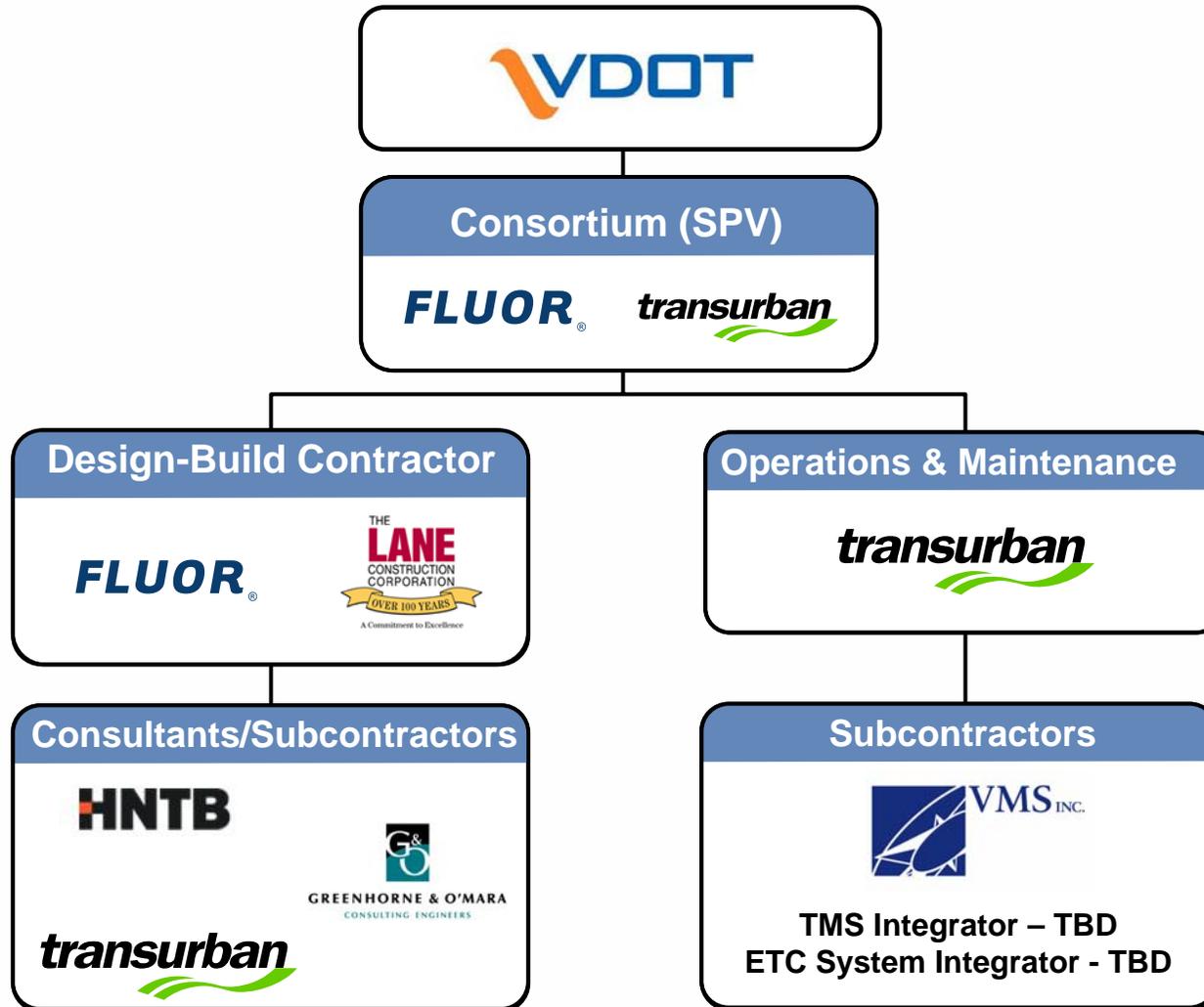


N North Access only
 N&S Full North-South Connection
 S South Access only

Enhancing the existing Capital Beltway



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Design-Build Process

- Design-Build Contract
- Large Project
- Aggressive Project Schedule
- Design Approximately 30 Percent Complete
- Involvement of Key Subcontractors

Design-Build Project Scope

- Construct 14 miles of HOT Lanes
- Add new lanes on both Inner and Outer Loop
- Increase capacity from 8 to 12 lanes
- Upgrade 12 key interchanges
- Construct more than 70,000 linear feet (13 miles) of new or upgraded sound walls
- Add, upgrade or replace aging infrastructure, including 58 bridges and overpasses

Project Budget/Schedule

- Estimated Project Budget: \$1.3 Billion
(subject to negotiations)
- Financial Close w/VDOT: Q4 2007
- Project Start: Q4 2007
- Construction Start: Spring 2008
- Construction Completion: 2012

Design-Build Procurement Approach

- Construction Subcontracts:
 - Typically unit price
 - Typically competitive bid
 - Require Insurance & Bonding

Fluor-Lane Subcontract Packages

1	Design Services (HNTB)
2	Construction QA/QC Services
3	Construction Survey Services
4	Asphalt Paving
5	Bridge Construction
6	Soundwall Construction
7	Permanent Signs
8	Steel Erection
9	Demolition (Bridges)
10	Demolition (Soundwalls)

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Fluor-Lane Subcontract Packages

11	Demolition (Pavement)
12	Demolition (Minor Buildings)
13	Wet Utilities (Waterlines)
14	Wet Utilities (Sanitary Sewer)
15	Intelligent Traffic Systems (ITS)
16	Roadway Lighting
17	Electrical
18	Pavement Marking
19	Rebar Placement
20	Operations Center Building Construction

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Fluor-Lane Subcontract Packages

21	Drilled Shafts
22	Erosion & Sediment Control
23	Traffic Signals
24	Clear & Grub
25	Fencing (ROW)
26	Fencing (Bridges)
27	Drainage
28	Pipe Jack & Bore
29	Minor Site Concrete
30	Concrete Recycling

Some things can't wait for traffic.

Fluor-Lane Subcontract Packages

41	Retaining Walls (Soil Nail)
42	Excavation
43	Shotcrete
44	Underdrain
45	Shoring & Support of Excavation
46	Piling
47	Bridge Deck Drains
48	Trucking

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Fluor-Lane Material Packages

1	Aggregates
2	Bridge Bearings/Expansion Joints
3	Readymix Concrete
4	Reinforced Concrete Pipe
5	Prestressed Concrete Girders
6	MSE Walls
7	Steel H-Piling
8	Rebar
9	Precast Drainage Structures
10	Precast Concrete Barrier

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Fluor-Lane Material Packages

11	Corrugated Metal Pipe (CMP)
12	Prefabricated Buildings
13	Traffic Control Devices
14	Sheetpile/Soldierpile
15	Underdrains
16	Formwork
17	Steel Fabrication
18	Portable Toilets
19	Office Materials & Supplies
20	Reproduction Services

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Fluor-Lane Material Packages

21	Site Office Cleaning Services
22	Fuel

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Fluor-Lane Expectations and Requirements

- Return DBE Questionnaire: Sept. 14, 2007
- Issue RFP Interest Letter: Q4 2007
- Return RFP Interest Letter: Q4 2007
- Issue RFP Packages: Post-Financial Close
- Prepare & Submit Proposals: Financial Close + 2 months
 - Insurance
 - Bonding
- Technical Assistance – Steve C. Kennedy
 - Insurance
 - Bonding
 - DBE/SWaM Certification

Some things can't wait for traffic.

Fluor-Lane Contacts

Project Information Mitch Lester mitch.lester@fluor.com 571-527-3602	DBE/SWaM Coordination Steve C. Kennedy SKennedy@laneconstruct.com 703-462-8145
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Project Web Site

www.virginiahotlanes.com

I-95/395 Project

Jeff Cole

Project Development Manager, Fluor Virginia, Inc



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I-95/395

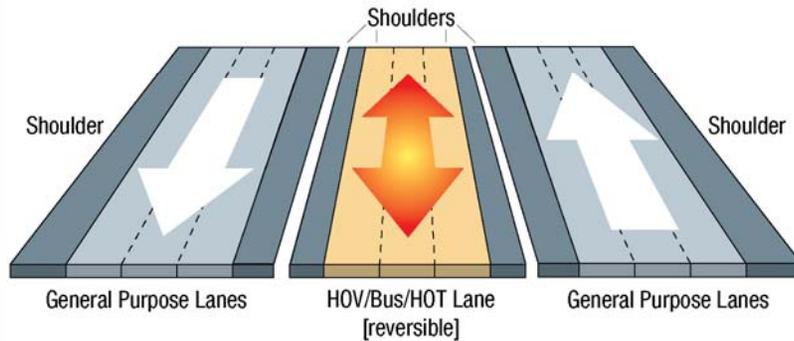
- Converts existing HOV lanes to HOT lanes
- Expands and extends existing HOV lanes
- 33 entry & exit points for better access to key activity & employment centers (Shirlington, Fort Belvoir)
- Provides safety, design and operational improvements to promote continued success of HOV facility
- Generates funding for public transportation improvements
- Split into two phases for environmental review, construction



I-95/395 lane configuration

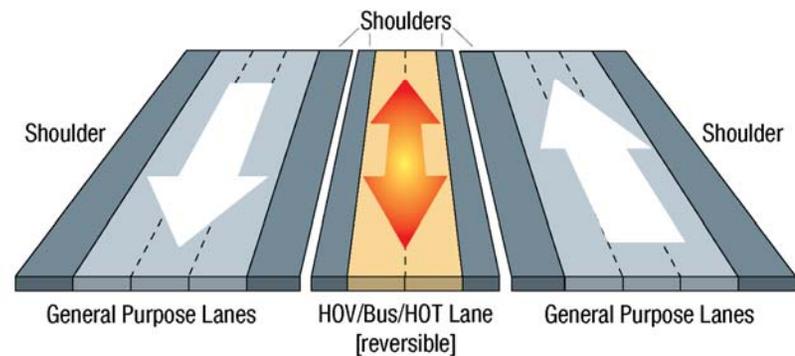
Northern Section: Interstates 95/395 HOV/Bus/HOT Lanes

*Eads Street (Arlington County) south to
Dumfries Road (Prince William County)*



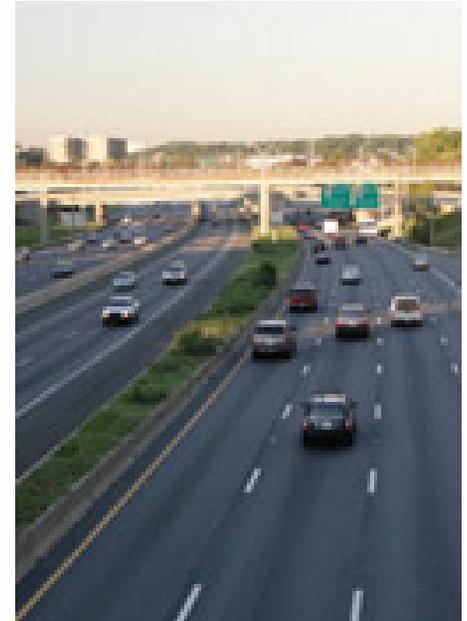
Southern Section: Interstates 95/395 HOV/Bus/HOT Lanes

*Dumfries Road (Prince William County) to
Massaponax (Spotsylvania County)*



Enhancing HOV

- Access to more places
- Enhanced enforcement
- Relief from bottlenecks
- Improved safety and incident response
- Increased capacity
- Faster, safer, more reliable travel
- Opportunities and incentives for sluggers



Improving public transportation

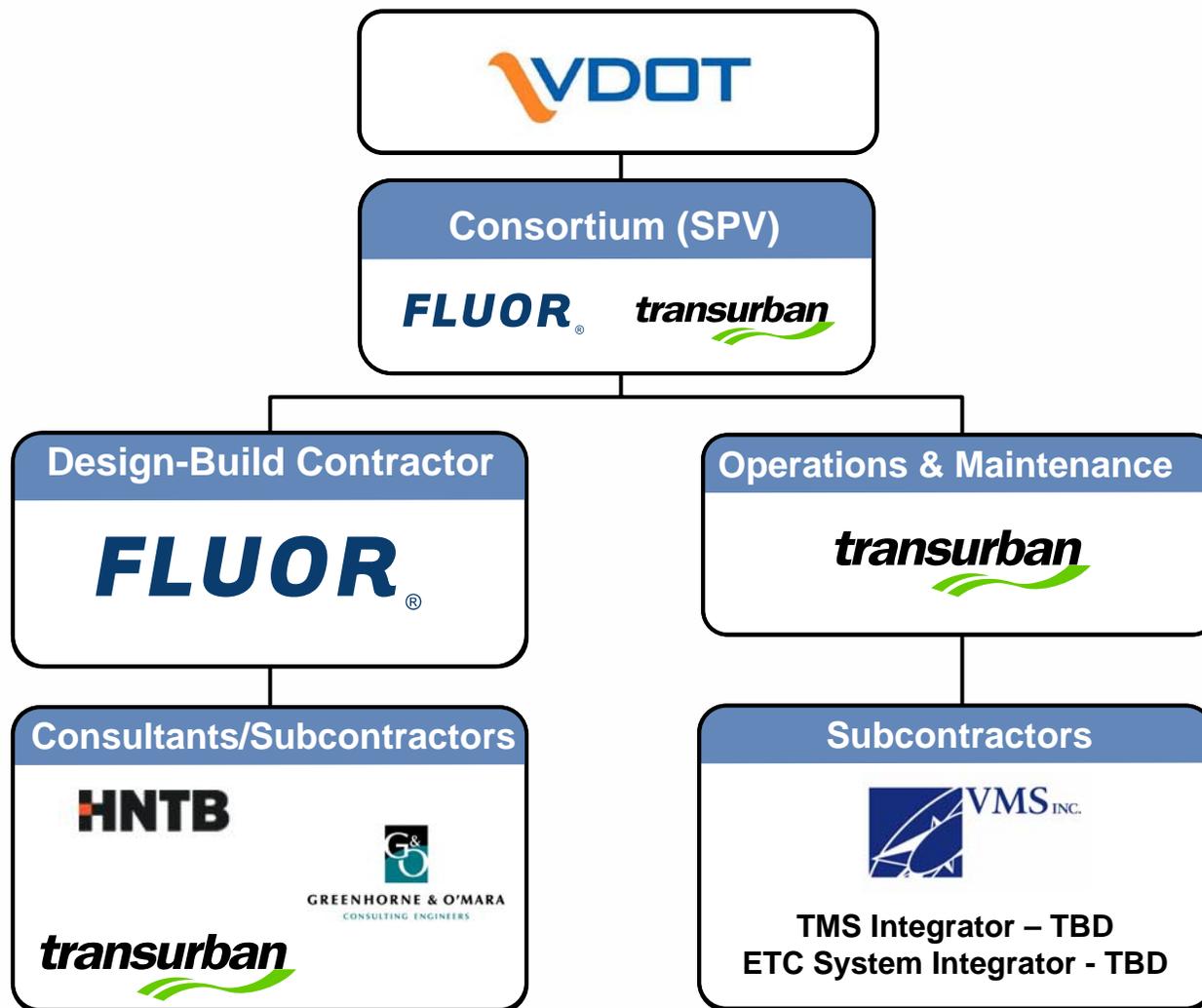
- **New infrastructure**
 - 56-mile, free-flowing network for buses
 - New entry & exit points
 - Bus station improvements, new Park & Rides
 - Bus-only ramps
- **New resources**
 - Project designed to generate excess revenue the Commonwealth can direct to transit improvements
 - Transit Advisory Committee regional transit/TDM study



I-95/395: where we are today

- October 2006: Signed Interim Agreement with VDOT
- Early stages of evaluation and input process
 - Constrained Long Range Plan (CLRP)
 - November 2006: VDOT launched federal environmental review process
 - Public meetings held Summer 2007
 - Continued stakeholder input and refinement of scope
- Projected construction start milestones
 - Northern section, 2009
 - Southern section, 2009/10

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Design-Build Process

- Design-Build Contract
- Large Project
- Aggressive Total Project Schedule
- During Development, will design the project to plans ready for Detailed Estimate
- Involvement of Key Subcontractors in Estimate

Design-Build Project Scope

- Construct 56 miles of HOV/Bus/HOT Lanes
- Increase capacity by adding one additional lane in the Northern Section and two additional lanes in the Southern Section
- Providing 33 entry and exit points within the corridor for the HOT Lanes
- Providing flyover ramps at key locations to exit the HOT lanes

Project Budget/Schedule

- Estimated Project Budget: \$1 Billion
(subject to negotiations)
- Financial Close w/VDOT: 2008
- Project Start: 2008
- Construction Start: 2009 (North)
- Construction Completion: 2011 (North)

Design-Build Procurement Approach

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 - Typically unit price
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Fluor Subcontract Packages

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Fluor Subcontract Packages

31	Seeding
32	Guardrail
33	Steel Girder Welded Studs
34	Bridge SIP Decking
35	Bridge Deck Grooving
36	Steel Girder Field Painting
37	Retaining Walls (MSE)
38	Retaining Walls (Cast In Place (CIP))
39	Retaining Walls (Soldier Pile)
40	Retaining Walls (Post & Panel)

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Fluor Contacts

Project Information Michael Hatchell Michael.hatchell@fluor.com 571-527-3601	Jeff Cole Jeffrey.cole@fluor.com 571-527-3607
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Project Web Site

www.virginiahotlanes.com

Questions

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