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CHAPTER 19

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* Indicates 11 x 17 sheet; all others are 8½ x 11.

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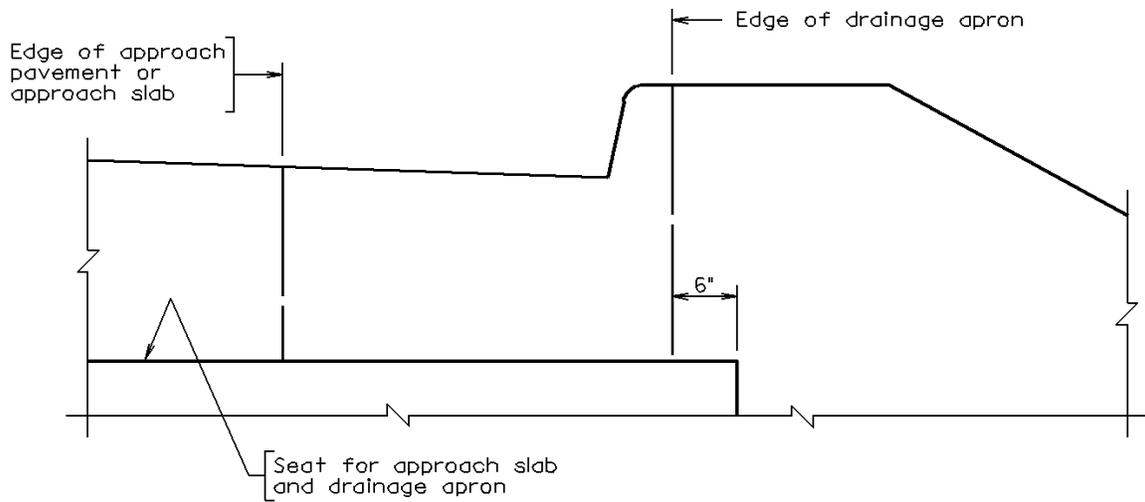
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This chapter is under development and will be revised at a later date.

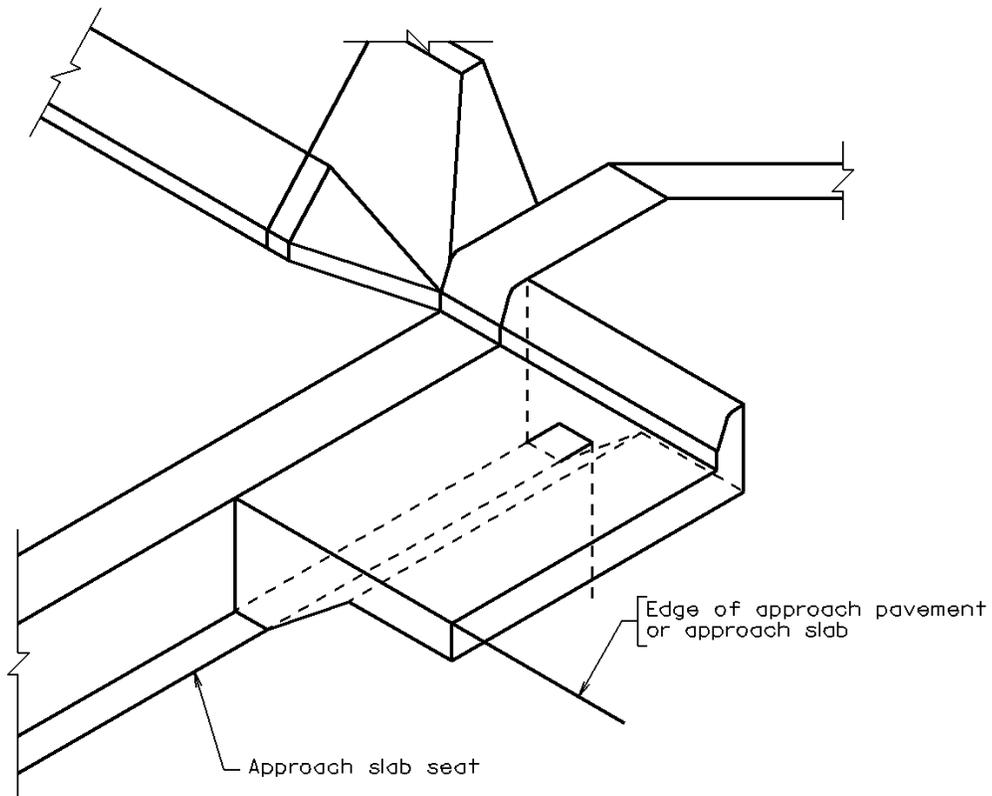
Data in this chapter is provided for use until revisions are complete.

**APPROACH SLABS
INTRODUCTION – CHAPTER 19**

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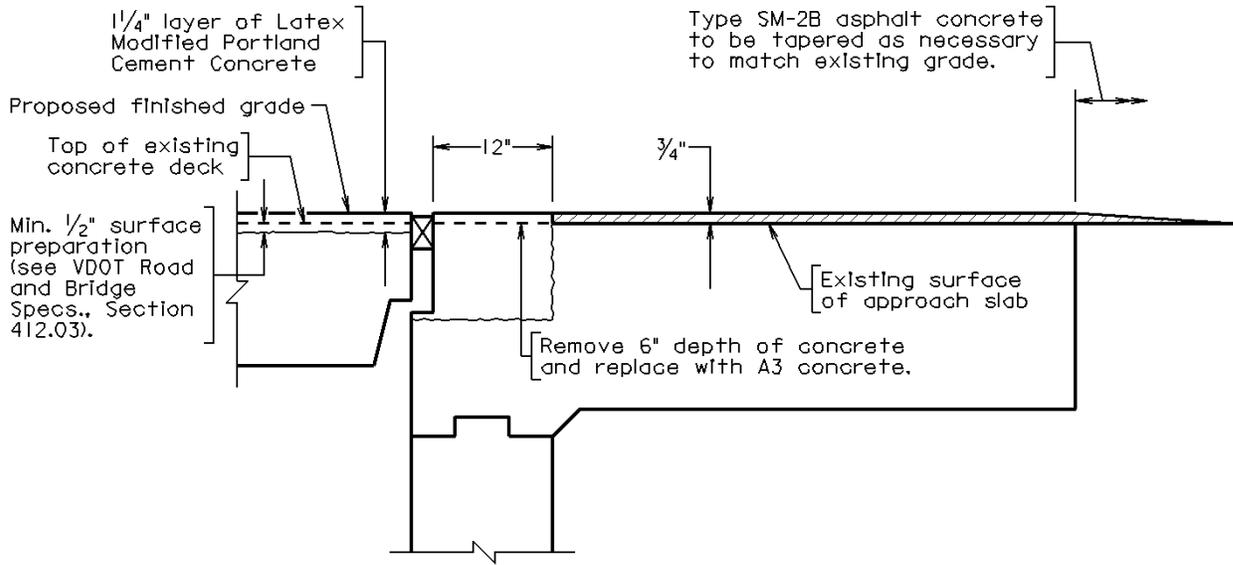
ELEVATION



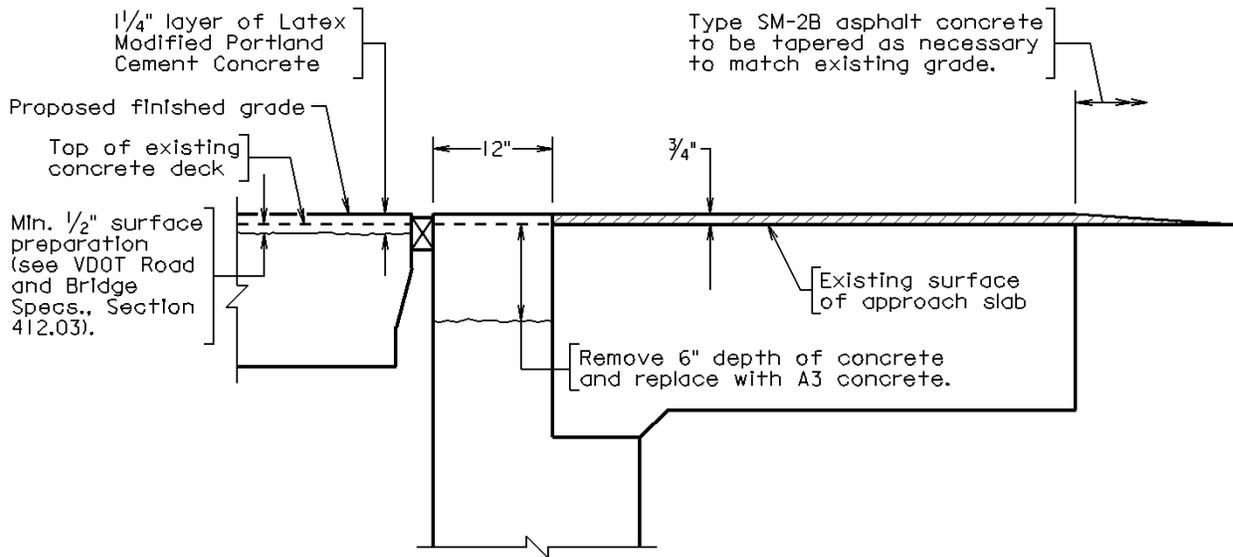
ISOMETRIC VIEW

**APPROACH SLABS
SEAT FOR APPROACH SLAB AND DRAINAGE APRON**

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APPROACH SLAB SUPPORTED ON BACKWALL

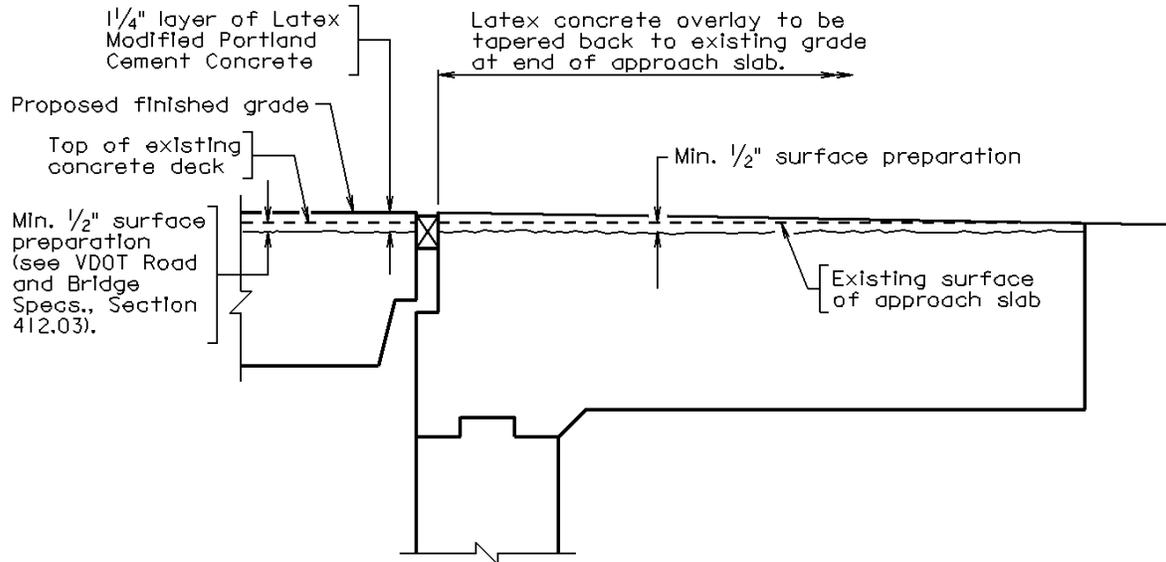


APPROACH SLAB SUPPORTED ON SHELF BEHIND BACKWALL

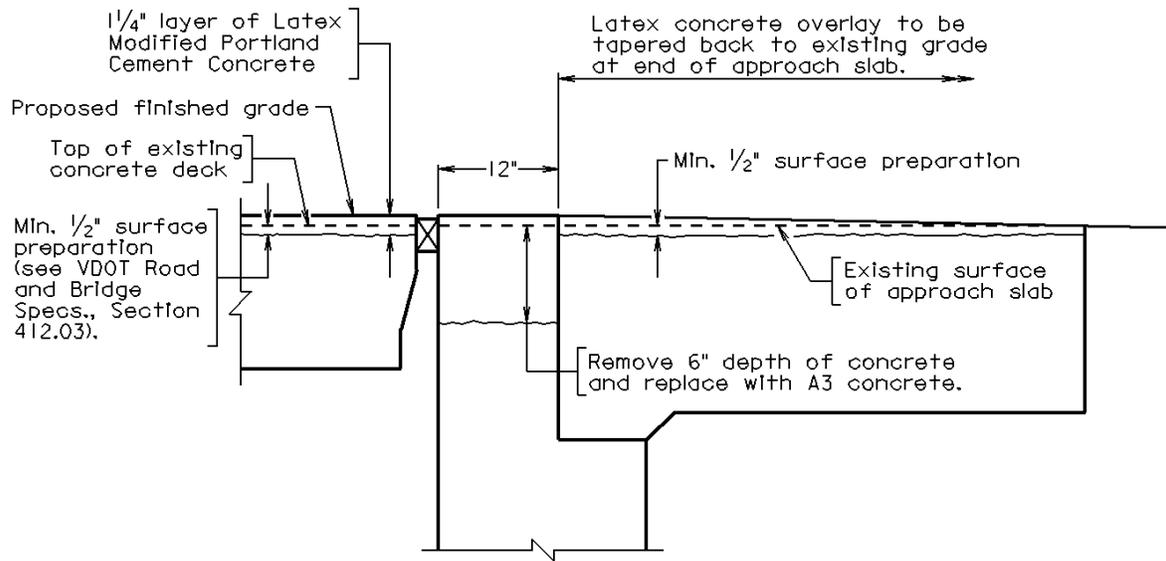
(FOR USE WITH BITUMINOUS CONCRETE APPROACH PAVEMENT)

**APPROACH SLABS
TRANSITION FOR LATEX PORTLAND CEMENT CONCRETE**

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APPROACH SLAB SUPPORTED ON BACKWALL

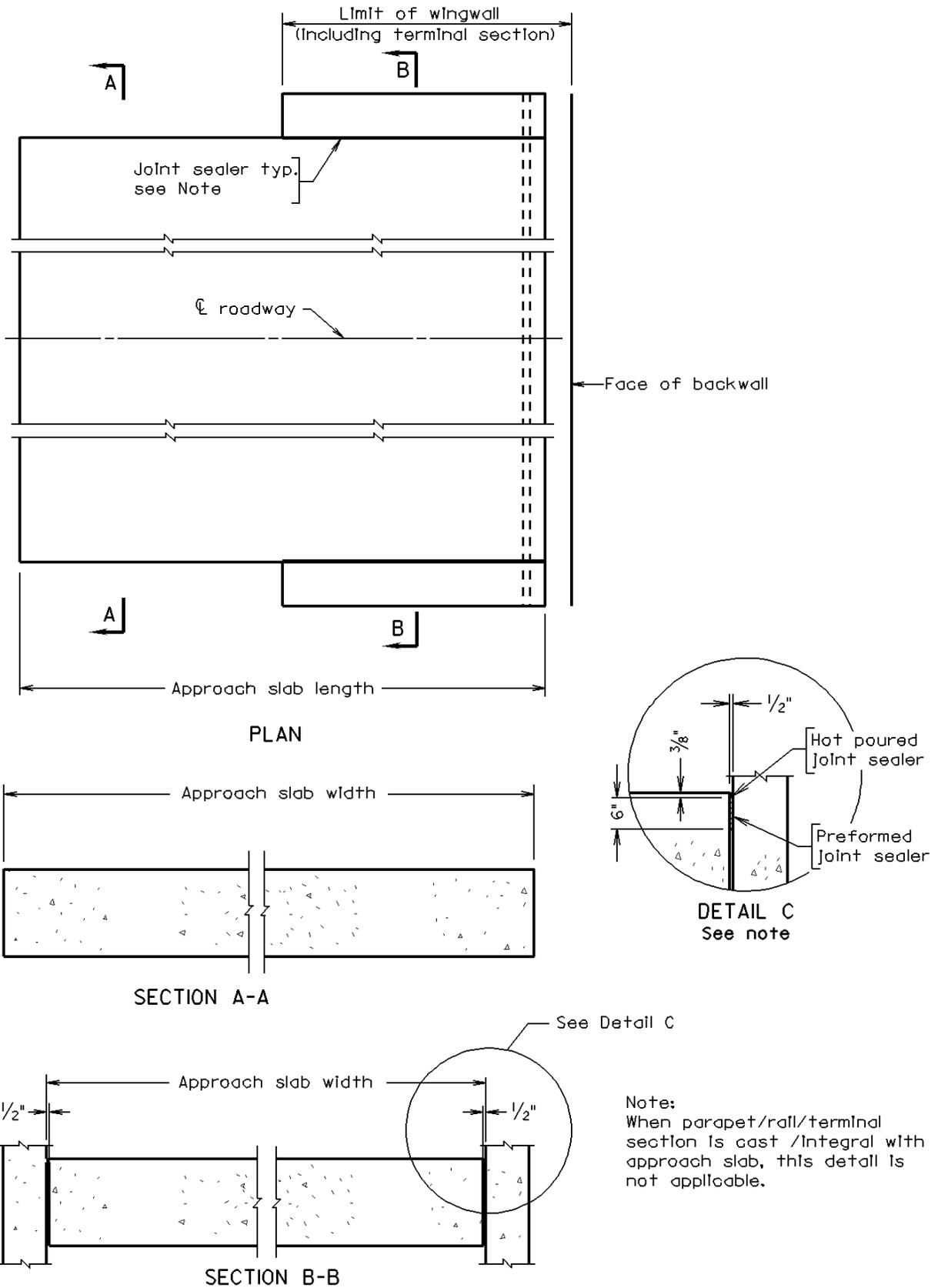


APPROACH SLAB SUPPORTED ON SHELF BEHIND BACKWALL

(FOR USE WITH CONCRETE APPROACH PAVEMENT OR WHEN APPROACH SLAB REQUIRES SURFACE REPAIR)

**APPROACH SLABS
TRANSITION FOR LATEX PORTLAND CEMENT CONCRETE**

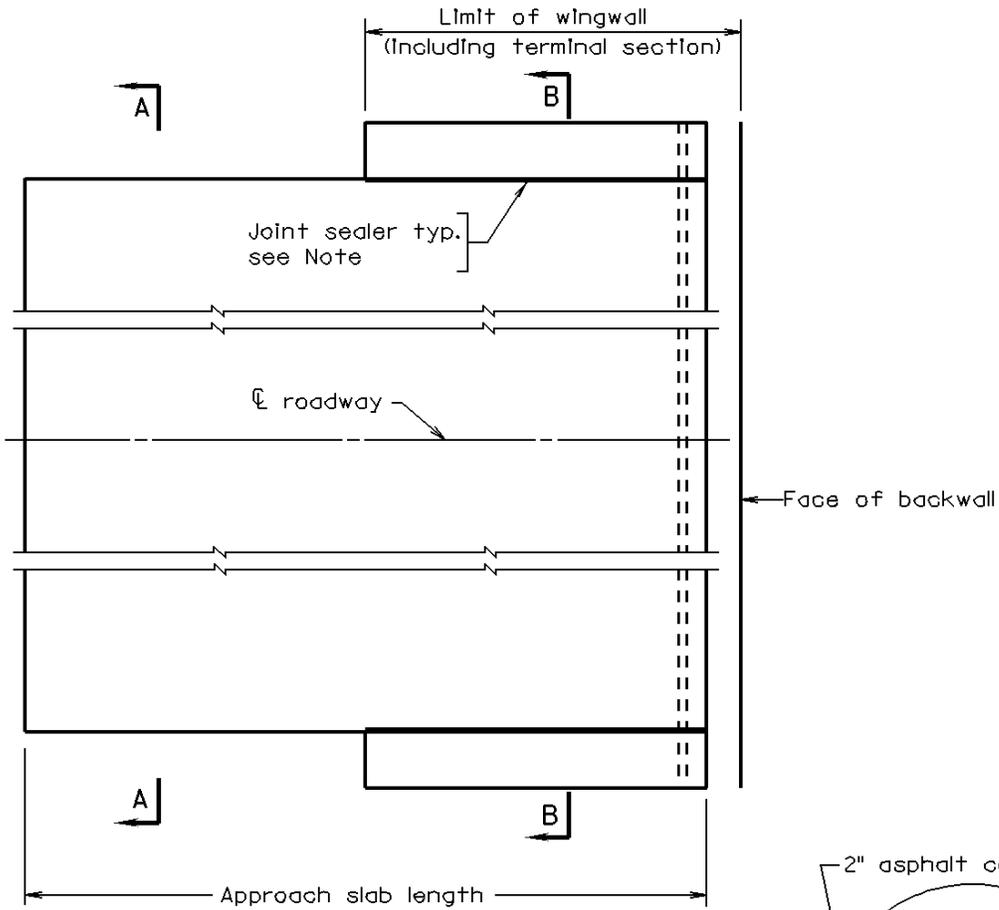
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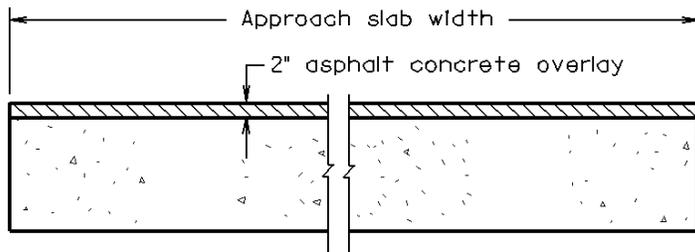
Note:
 When parapet/rail/terminal section is cast /integral with approach slab, this detail is not applicable.

APPROACH SLABS
FULL WIDTH APPROACH SLABS
APPROACH SLAB W/O CURBS OR OVERLAY

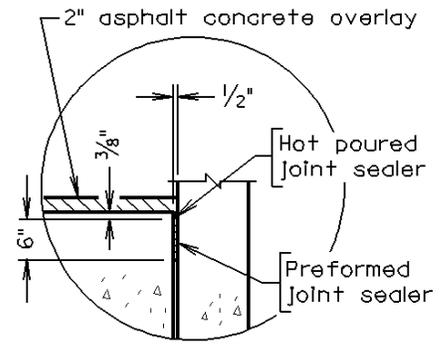
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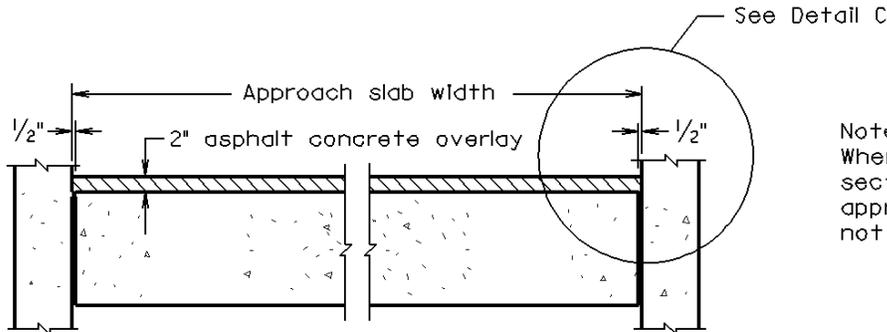
PLAN



SECTION A-A



DETAIL C
See note

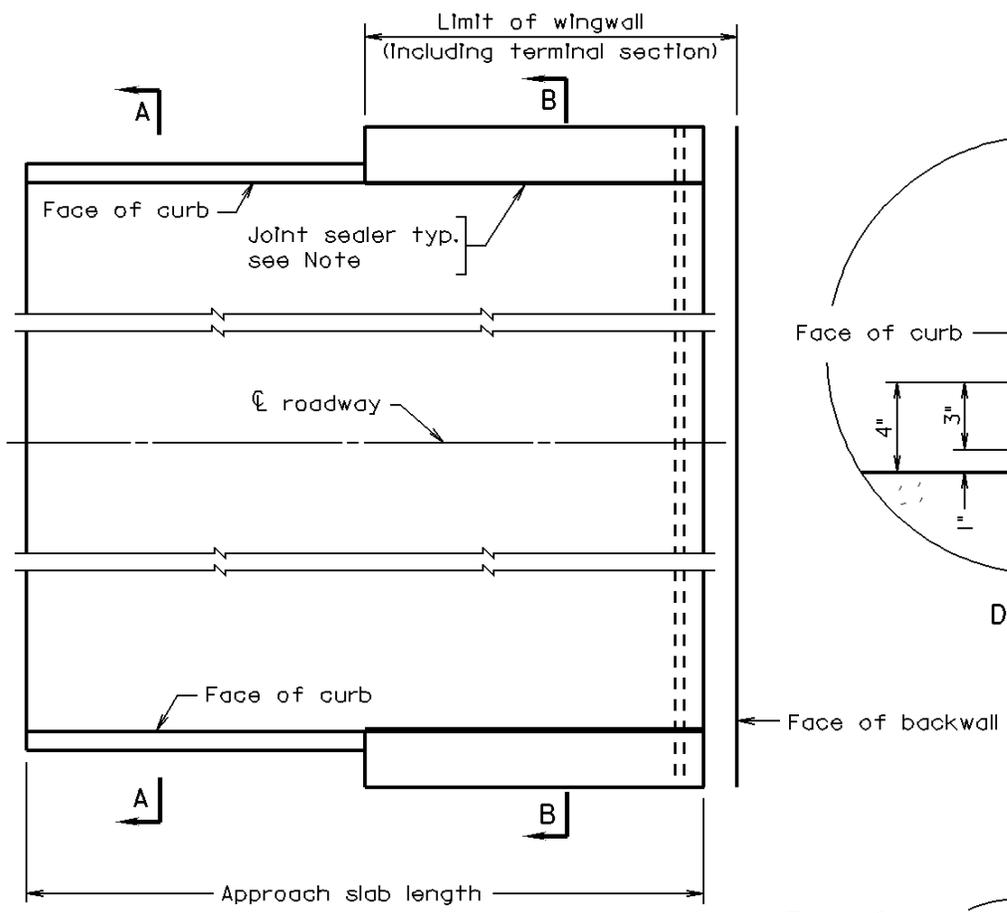


SECTION B-B

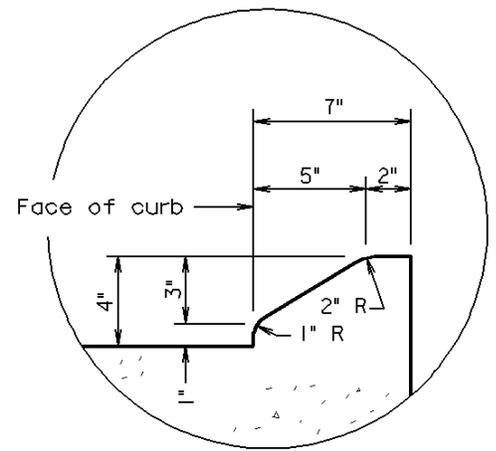
Note:
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APPROACH SLABS
FULL WIDTH APPROACH SLABS
APPROACH SLAB W/OVERLAY AND W/O CURBS

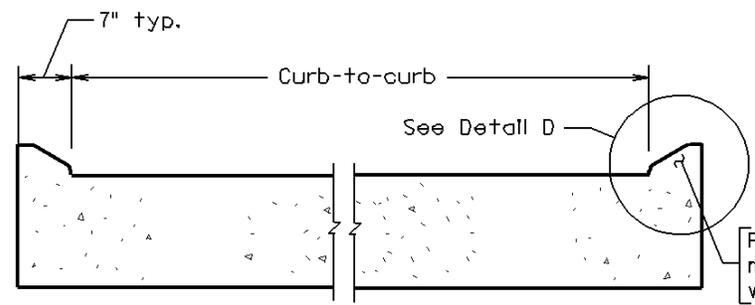
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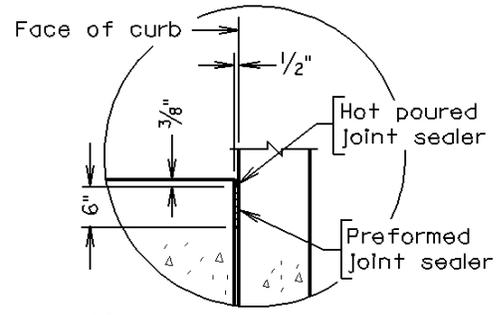
PLAN



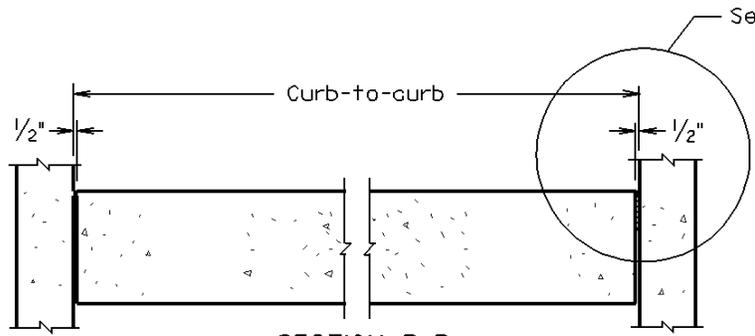
DETAIL D



SECTION A-A



DETAIL C
See note

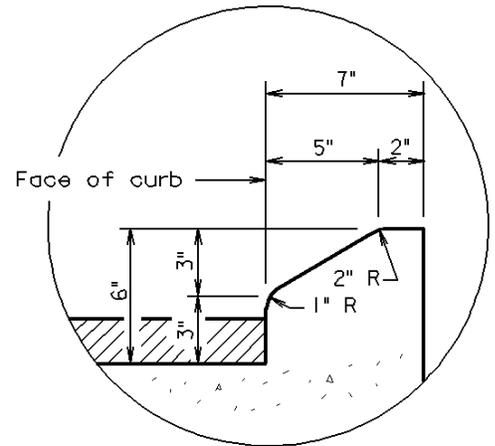
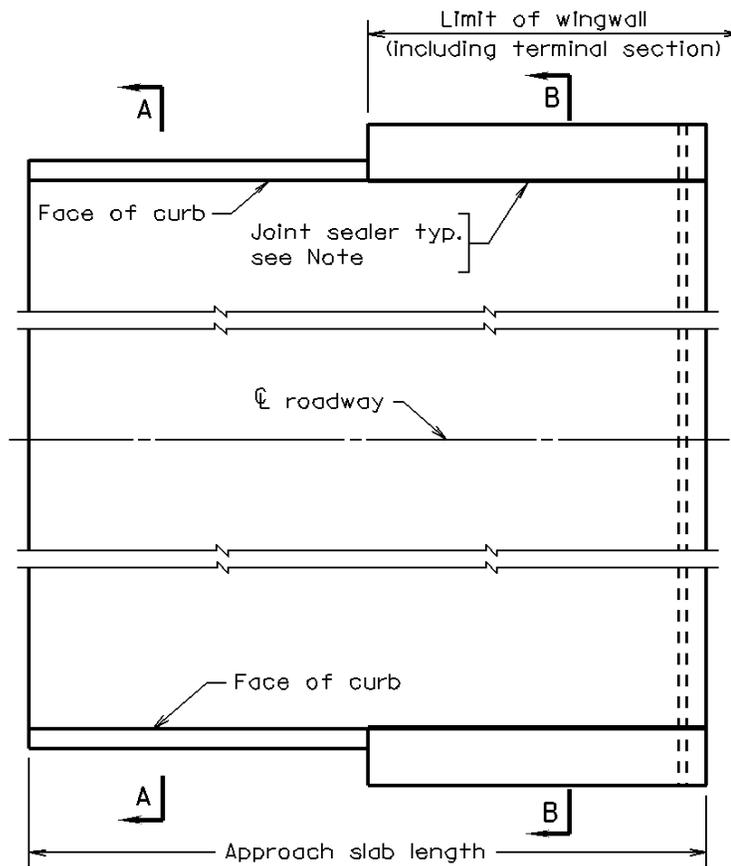


SECTION B-B

Note:
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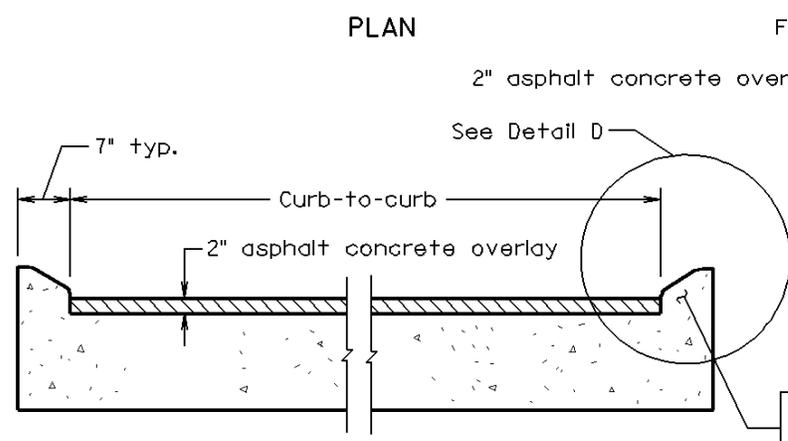
APPROACH SLABS
FULL WIDTH APPROACH SLABS
APPROACH SLAB W/CURBS AND W/O OVERLAY

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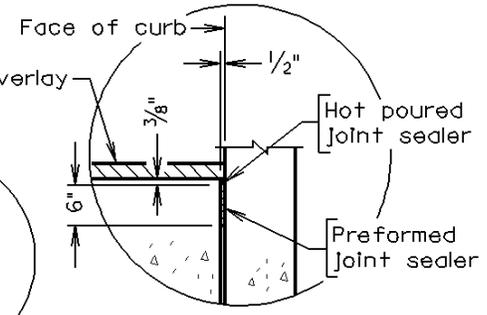


DETAIL D

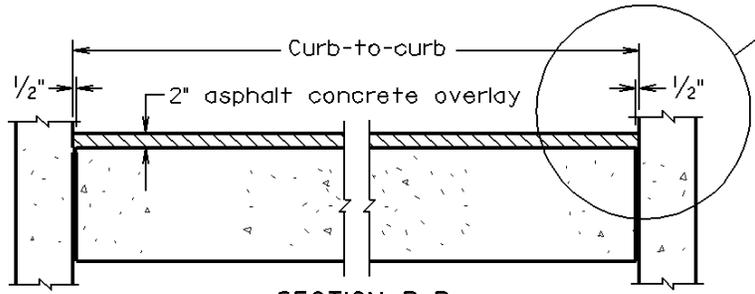
PLAN



SECTION A-A



DETAIL C
See note



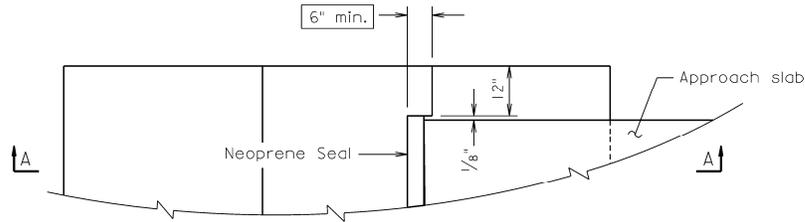
SECTION B-B

Note:
When parapet/rail/terminal section is cast integral with approach slab, this detail is not applicable.

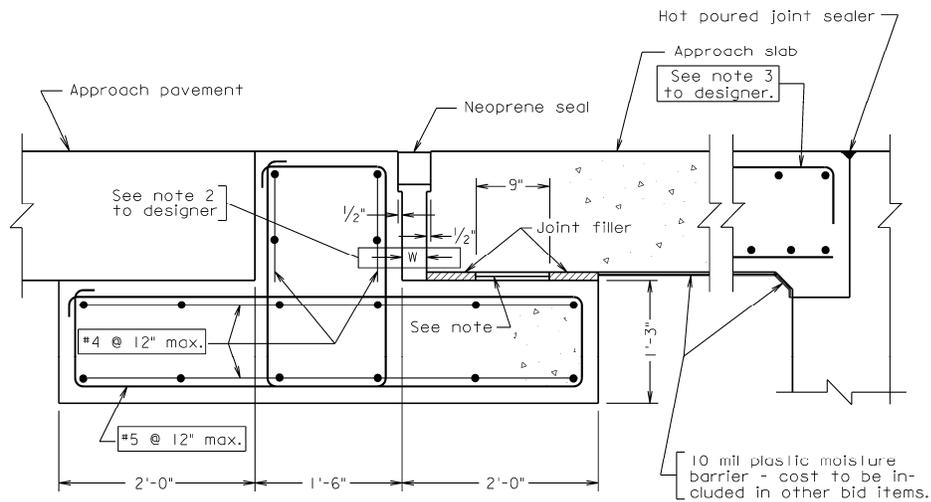
APPROACH SLABS
FULL WIDTH APPROACH SLABS
APPROACH SLAB W/ CURBS AND OVERLAY

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APPROACH SLAB SLEEPER PAD DETAILS:



SLEEPER PAD PART PLAN



SECTION A-A

1. Items in blocks are for designer's information only and are not to be placed on the plans.
2. $W = \text{total thermal movement range due to expansion and contraction} + \frac{1}{4}''$.
3. When the approach slab is used with integral bridge abutments, modify approach slab reinforcing steel to have a 90° hook as shown. Modify the above shown sleeper pad as required for skewed bridges.
4. 2 - $\frac{1}{8}''$ preformed bedding material pads conforming to Section 237 of the current VDOT *Road and Bridge Specifications*, for full length of approach slab seat. Between the pads use either a powdered graphite lubricant, Molykote 321 by Dow Corning or Mantek Dri-guard.