



2019 UCI ANNUAL MEETING

VDOT ITEMS OF INTEREST

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June 26, 2019

Today's Agenda

- **Legislative Update**
- **Special Funding Programs**
 - **Revenue Sharing**
 - **Transportation Alternatives**
- **Urban Maintenance Program**
- **SGR/Primary Extension**
- **Performance Reporting**
- **Other VDOT Initiatives**
- **Qualifications Program**

UCI ANNUAL MEETING

UCI LEGISLATIVE UPDATE

Legislative Updates

- **HB2084: BILL LEFT IN COMMITTEE**
 - Similar to HB302 of the 2018 GA session
 - Requires Counties with populations of 100,000 or over to take over maintenance responsibility of their roads and receive funding under urban code section.
- **SB1759: PASSED**
 - Undergrounding utility lines pilot program; transportation infrastructure improvement; urban county executive form of government
 - Establishes a pilot program under which the governing body of any locality operating under the urban county executive form of government (Fairfax County) may request an electric utility to place underground electric utility distribution lines in transportation projects to serve and facilitate the creation of transit-oriented development in such locality in conjunction with a transportation infrastructure improvement project that the Commonwealth Transportation Board identifies that reduces congestion, improves mobility, incorporates transit systems and improves safety.

Legislative Updates (Continued)

- **SB1421: Passed**
 - **Eminent domain; entry upon private property; calculation of just compensation; damages.** Makes various changes to provisions related to entry upon private property in an eminent domain proceeding, including (i) requiring that the number of persons for whom permission to inspect the premises is sought be included in a request for permission to inspect private property for the purposes of a project wherein the power of eminent domain may be exercised; (ii) requiring the notice of intent to enter the property to include all of the information contained in the request for permission to inspect the property; (iii) requiring the court to award fees for up to three experts or as many experts as are called by the petitioner at trial, whichever is greater, if the petitioner damages the property during its entry; (iv) removing the requirement that the damage must be done maliciously, willfully, or recklessly for the owner to be reimbursed for his costs; and (v) removing the option that the owner may be reimbursed for his costs if the court awards the owner actual damages in an amount 30 percent or more greater than the petitioner's final written offer made no later than 30 days after the filing of an answer in circuit court or the return date in general district court.

Legislative Updates (Continued)

Budget Bill Amendments: APPROVED

- **SEAT OF GOVERNMENT TRAFFIC AND PEDESTRIAN SAFETY (HB1700 – Item 4-5.12).**
 - In order to implement and maintain traffic and pedestrian operational safety and security enhancements and secure the seat of government, the CTB shall, not later than January 1, 2020, add to the state primary highway system, various portions of the rights-of-way located in the City of Richmond pursuant to the responsibilities of the Department of General Services (DGS) and the Division of Capitol Police (DCP), DGS and DCP shall control those rights-of-way and pedestrian and vehicular traffic thereon. The rights-of-way so transferred shall be in addition to the 50 miles per year authorized to be transferred under § 33.2-314(A).

Past Approved Legislation Update

REMINDER:

- **HB 2023 – Payments to Cities and Towns for moving-lanes converted to bicycle lanes**
 - Moving-lane miles converted after July 1, 2014
 - Requires Professional Engineer Certification
 - Conversions are limited to 50 lane miles or 3% of the municipalities total lane miles
 - Updated Chapter 2.5.3 of the Urban Construction and Maintenance Policy and Guidance Manual (aka Urban Manual)
 - Reminder that any conversions of thru lanes receiving payments to bicycle or transit-only must follow the U-1 process outlined in the Urban Manual

UCI ANNUAL MEETING LAD PROGRAMS UPDATE

Local Programs Administered by Local Assistance Division

Revenue Sharing	Access Programs (EDA, Rec, Airport)	Transportation Alternatives	Rural Rustic Roads
Outreach	Local System (Urban/ Secondary)	Federal Lands Access Program	SGR/Primary Extension/High Volume Unpaved
Urban Construction Initiative	Compliance Assessments	Locally Administered Projects/Programs	Misc Coal Severance ARC Access VA Byways

- + Safe Routes to School (SRTS) – Transportation and Mobility Planning
- + Highway Safety Improvement Program (HSIP) – Traffic Engineering

Revenue Sharing Program Overview

Program Facts:

- **50/50 matching program**
- **Open to Counties and Cities and Towns in the Urban System**
- **Program allocation up to \$100 million**
- **Projects prioritized based on Code**
 - Project previously received Revenue Sharing funds
 - Project meets Statewide Transportation need or receipt of funding will accelerate project in locality's capital plan
 - Project addresses pavement or bridge deficiency
- **Locality Limit \$5M/Year-Lifetime Project Limit \$10M**
- **Stricter Transfer Limits in Place**
- **Policy modifications resulting in more deallocations available statewide**

FY 2020 Update to CTB

- **De-Allocated Funds Re-Cap:**
 - **January 2019 Deallocation Amount:** **\$16,899,462**
 - **Amount to Fund Priority 2 Pro-rated Projects:** **\$ 6,898,128**
 - **Amount to Fund all Priority 3 Projects:** **\$ 5,719,500**
 - 7 projects in 7 localities in 3 Districts
 - **Revenue Sharing Balance Entry Remaining:** **\$ 4,281,834**

Remaining balance entry funds will be used for re-distribution as part of the FY21/FY22 Revenue Sharing Program.

Surface Transportation Block Set-Aside Grant Program (Transportation Alternatives)

- ▶ Intended to improve non-motorized transportation, enhance the public's travel experience, revitalize communities and improve the quality of life.
- ▶ Application Deadline – October 1st every (now on a 2-year cycle)



Transportation Alternatives Program

- Available funding includes Transportation Alternatives (TA), Safe Routes to Schools (SRTS), and a Recreational Trail Programs mandatory set-aside
- Federal-aid
- 10 eligibility categories
- The program requires a 20% local match
- Donations for match acceptable (upon VDOT review and approval) and can include professional services, land value, and donated materials
- These are typically locally administered projects
 - VDOT is modifying process to allow for more VDOT administration

FY19/20 Application Summary - TA

<i>Allocation Distribution</i>	
MPO/TMA Areas	<i>\$12.2M</i>
District Members	<i>\$18M (\$2M per District)</i>
At-Large Members/Secretary	<i>\$10M</i>
Total	<i>\$40.2M</i>

TAP Emphasis Areas

Project Progress and Completion

CTB Policy for Construction within four years of allocation

FHWA Regulation requiring advancement within 10 years (10-year Rule)

Projects will need justification and possible FHWA concurrence to continue and avoid payback

Project Scope Creep/Modifications

Tendency for TA Projects to Change Scope and Modify beyond original application request

Federal Approval is based on Submitted Scope

Future Initiatives

Improve Dashboard/Completion Rates

Policies to improve Delivery

Ensure Completed Projects continue to serve intended use

Pre-Application Process



Pre-application submission ****MANDATORY****

- Portal opens May 15, 2019 at 8am
- From May 15 – July 1 will be a “rolling submission” period
- Locality is “owner” of the pre-application and can make changes until it is SUBMITTED
- Districts will be more involved in Application Reviews and Emphasis this year is on better estimates and scope
- Portal closes to pre-applications July 1, 2019 at 5pm
- Final Applications due October 1, 2019 at 5pm

Pre-Applications So Far*

TOTAL PRE-APPLICATIONS			
TA Created	TA Submitted	RS Created	RS Submitted
95	6	132	23

Creating the Application is not enough!

* 6/24/19

FAST Act Rescission

\$7.569 Across Many Program Areas Across All States

Reduces Unobligated Balances across Funding Categories

“competition across states”

Outreach: November 6, 2018 Commissioner Letter to Localities; VDOT Presentations to MPOs; LAD Spring 2019 Newsletter

Local Assistance Division Impact: Transportation Alternatives

Impact TA Funding Categories Separately – TA Statewide and Population Areas

Strategies to Mitigate Impacts

Urban Maintenance Program Local Maintenance Payments

Eligibility Requirements for Maintenance Payments:

- Urban street acceptance criteria established in Code Section 33.2-319
- CTB approves mileage additions/ deletions

Payment - General

- Payments based on moving lane miles (available to peak-hour traffic)
- CTB approves payment amounts to localities
- Localities annual growth rate is based upon the base rate of growth for VDOT's maintenance program
- Payments to localities made quarterly

Payment Categories – Based on Functional Classifications

1. Principal and Minor Arterial Roads
2. Collector Roads and Local Streets

FY20 Urban Local Maintenance Payments

- Urban (84 Cities and Towns)
 - Overall Urban Budget ≈ \$ 387M
 - Payment Rates:
 - Principal and Minor Arterial Roads = \$21,689 per lane mile
 - Collector Roads and Local Streets = \$12,735 per lane mile
 - Arterial Lane Miles: 5,968
 - Collector/ Local Miles: 20,184
 - FY20 Overweight Permit Fee Urban Distribution ≈ \$179,780; Equivalent to \$6.87 per lane mile
- Continue \$1M to Chesapeake to address additional costs associated with movable bridges (payments began 2005)

Local Maintenance Program Adjustment

- This year's maintenance payment rates are equal to last year's rates
- There have been planned reductions in maintenance funding for the past three years. As is practice, the same growth factor is applied to VDOT Maintenance and Locality Maintenance Payments. The reductions planned in FY 2019 and 2020 were scaled back last year from the original plan.
- Initially presented to the CTB in January 2017, reductions were proposed to the Maintenance Program after applying CPI growth
 - FY 2018 \$25 million one-time
 - FY 2019 \$12 million one-time (reduced to \$2.26M)
 - FY 2020 \$75 million base reduction (reduced to \$67M)
- Adjustments were applied to the VDOT Maintenance Program and to City and County Street Maintenance Payments based on pro rata shares.
- Efforts have been made to lessen the impacts of these reductions which included an additional allocation in FY 2020 of \$2,504,018 to normalize this year's maintenance payment rates

State of Good Repair (SGR)/Primary Extension Paving Program Criteria

- **Accept applications on an annual basis to support pavement overlay, rehabilitation, or reconstruction projects**
 - **Maximum request of \$1M per locality, per year**
 - **Roadway must have Combined Condition Index (CCI) rating of less than 60**
 - **Projects must be advertised within 6 months of allocation**
 - **Maintenance of Effort Certification required**
- **Prioritize projects for funding based on technical score that considers pavement condition, traffic volume, and past expenditures**
 - **Pavement condition (CCI) – 45%**
 - **On the National Highway System (NHS) – 10%**
 - **Traffic volume – 30%**
 - **Prior expenditures – 15%**
- **Same criteria used to select projects for CTB Formula and State of Good Repair funding**
- **The SGR local program is currently exempt from the federal process until FY2021**

FY20 SGR/ Primary Extension Paving Applications: General Information

- **Call for applications on November 1, 2018**
- **All applications due by January 7, 2019**
- **Second year applications were entered using VDOT's Smart Portal**
- **Scored 122 applications with requests over \$30.3M**
- **Applications received from 45 localities representing all 9 districts**
- **Funding for locality's primary extensions available from both State of Good Repair (SGR) funds and CTB formula funds**
- **Prioritized and selected primary extension projects using SGR funds first followed by the CTB formula funds**
- **84 total paving projects will tentatively receive funding representing 35 localities**

FY20 - State of Good Repair (SGR) Funding for Primary Extensions

- Each District has a set percentage of SGR funding dedicated to Locality pavement projects
 - \$6.2M estimated available for FY20 local SGR pavement projects (Primary Extension)
 - Recommend funding projects in priority order until funding exhausted; based on current estimates this will:
 - Fully fund 22 local paving projects
 - Partially fund 9 local paving projects
- (Note: The 9 partially funded projects were also reviewed for CTB Formula Funding)

FY20 - CTB Formula Funding for Primary Extensions

- **Set aside of 14% of CTB formula funds for pavement designated for municipality maintained primary extensions**
 - **\$16 M available**
 - **Recommend funding projects in priority order until funding exhausted; based on current estimated CTB formula funds this will:**
 - **Fully funding the top 61 scoring remaining applications which includes 9 carryovers from SGR (Funds all remaining applications with total scores of 42.7 and above)**
 - Partially funded 1 project with a score of 42.2
- **Localities in all 9 districts receive additional funding**

SGR – Scoring Process – Bridges (Locally Owned)

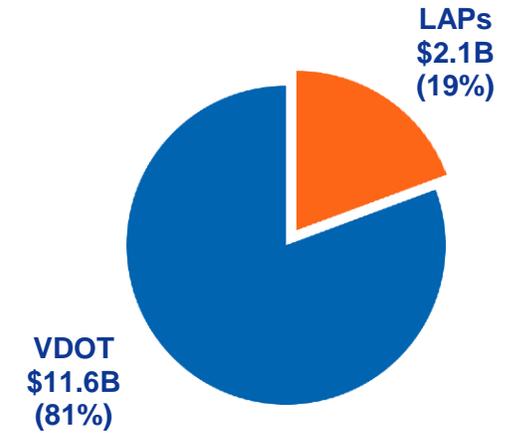
- **Accept applications on an annual basis to support bridge rehabilitation, or reconstruction projects**
 - **Bridge must be structurally deficient**
 - **National Bridge Inventory Only**
 - **The bridge must not have been replaced or undergone a deck and/or superstructure replacement during the ten (10) years prior to the date of application**
 - **Proposed work must take bridge out of structurally deficient status**
 - **Localities must be current on bridge inspections**
 - **Starting in 2021, the prior fiscal year bridge maintenance expenditures, as reported to the Weldon Cooper Center, to be a minimum of 2%**
 - **Project receiving funding under this program must initiate the Preliminary Engineering or the Construction Phase within 24 months of award of funding or become subject to deallocation**
- **Prioritize projects for funding based on technical score that considers bridge prioritization and cost effectiveness**

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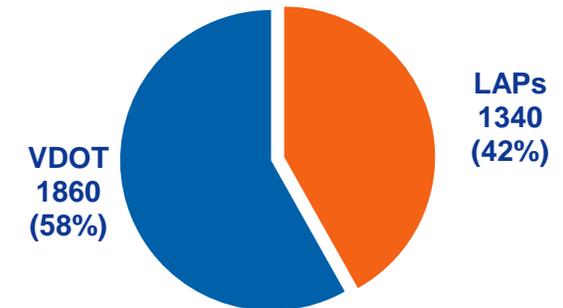
Statewide LAP Program Snapshot

District	Projects	% of LAP CN Projects	CN \$	% of LAP CN \$
Bristol	154	4.8%	\$ 95,518,037	0.7%
Culpeper	66	2.1%	\$ 98,962,052	0.7%
District-Wide	2	0.0%	\$ 175,692	0.0%
Fredericksburg	38	1.2%	\$ 28,866,613	0.2%
HR	276	8.6%	\$ 804,560,177	5.6%
Lynchburg	74	2.3%	\$ 63,912,772	0.4%
Northern VA	274	8.6%	\$ 994,816,516	6.9%
Richmond	213	6.7%	\$ 425,071,715	3.0%
Salem	104	3.3%	\$ 144,462,506	1.0%
Statewide	12	0.4%	\$ 2,162,727	0.0%
Staunton	127	4.0%	\$ 123,005,953	0.9%
Total	1340	42%	\$2,781,484,760	19.3%

LAP Percent of CN \$\$
\$14.4B



LAP Percent of CN Projects
3200 TOTAL Projects



*Table values represent all current “Active” projects in VDOT’s Project Pool application

Dashboard Score by District

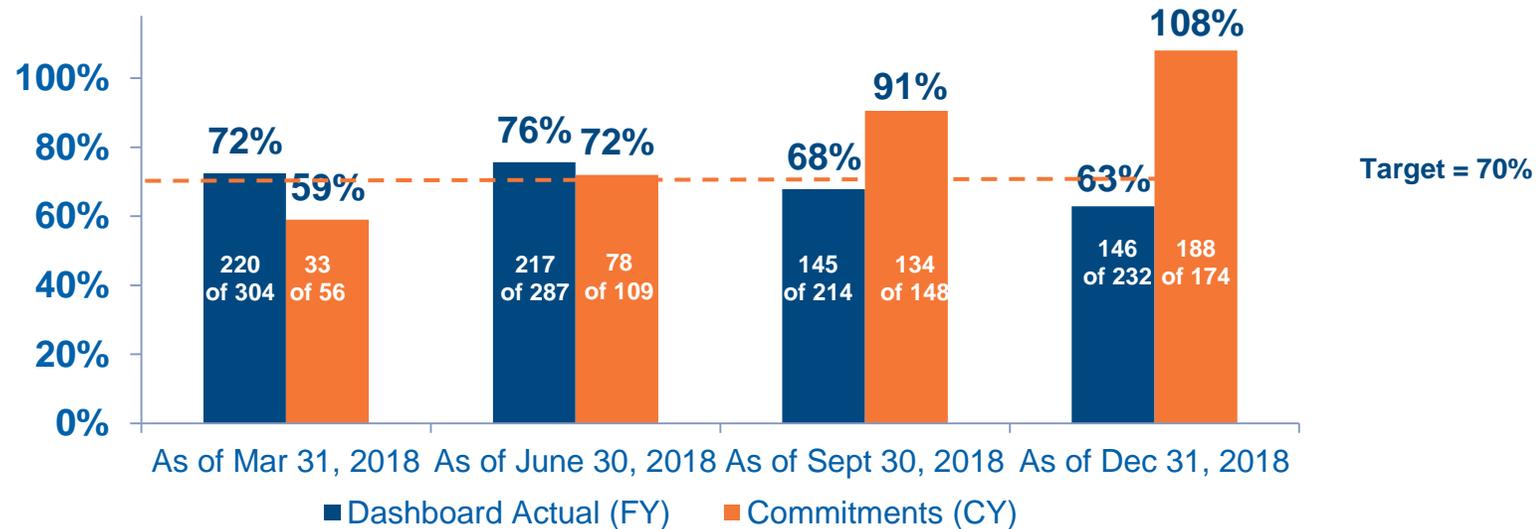
On Delivery Goal 70%

On Budget Delivery Goal 74%

As of 5/22/2019	On-Time Development	On-Budget Development
Bristol	67%	67%
Culpeper	59%	82%
Fredericksburg	75%	100%
Hampton Roads	65%	61%
Lynchburg	92%	83%
Northern Virginia	44%	70%
Richmond	68%	64%
Salem	74%	77%
Staunton	58%	67%
Statewide	64%	70%

Performance: On-time Development

Construction Advertisements



Dashboard	Commitments
<ul style="list-style-type: none"> Tracked and reported utilizing data available from the VDOT project/program management applications (Pool, iPM, Schedule, PCES, etc.) Data accuracy reliant on close communication between VDOT PC and LPA PM. 	<ul style="list-style-type: none"> Interactively and independently compiled and tracked on a calendar year basis to monitor advertisements. For CY19: 169 projects committed, having a construction value of \$387M.

Performance Reporting

Construction Advertisement Commitments

	Non-UCI Localities		UCI Localities		Combined	
	#	\$	#	\$	#	\$
CY2015	141%	108%	122%	61%	117%	93%
CY2016	98%	116%	73%	95%	90%	109%
CY2017	119%	152%	135%	123%	121%	134%
CY2018	103%	64%	108%	106%	104%	78%
CY2019 <i>(thru 6/15)</i>	130%	243%	135%	48%	131%	193%

Non-VDOT Administered Projects (NVAP)

NVAP Introduced July 1, 2018

Monitor LAP Development/Delivery

Initial UPC Data from VDOT POOL

Locality Enters UPC Specific Data

Data Displays on Dashboard “Projects”

Filter by UPC, Administered By, Status



UCI ANNUAL MEETING OTHER VDOT INITIATIVES

VDOT INITIATIVES

- **BUSINESS PLAN UPDATE**
- **PILOT FOR ELECTRONIC SIGNATURES FOR AGREEMENTS**
- **UTILITY COORDINATION**
- **SPECIAL HAULING VEHICLES/ EMERGENCY VEHICLE POSTING**
- **PRE-AWARD EVALUATION TEMPLATES**
- **EXTERNAL ACCESS TO VDOT SYSTEMS**
- **QUALIFICATIONS PROGRAM**

Local Programs Workshop



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SMART SCALE Dashboard

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Network for Success Local Programs Workshop



Save the Date! The 2019 Local Programs Workshop will be September 9-11, 2019 at the Williamsburg Lodge. [Register here!](#)

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QUALIFICATIONS PROGRAM UPDATE